



# Beacon 248 Development City of Beacon, New York Dutchess County

Presented by:

Cuddy and Feder LLP

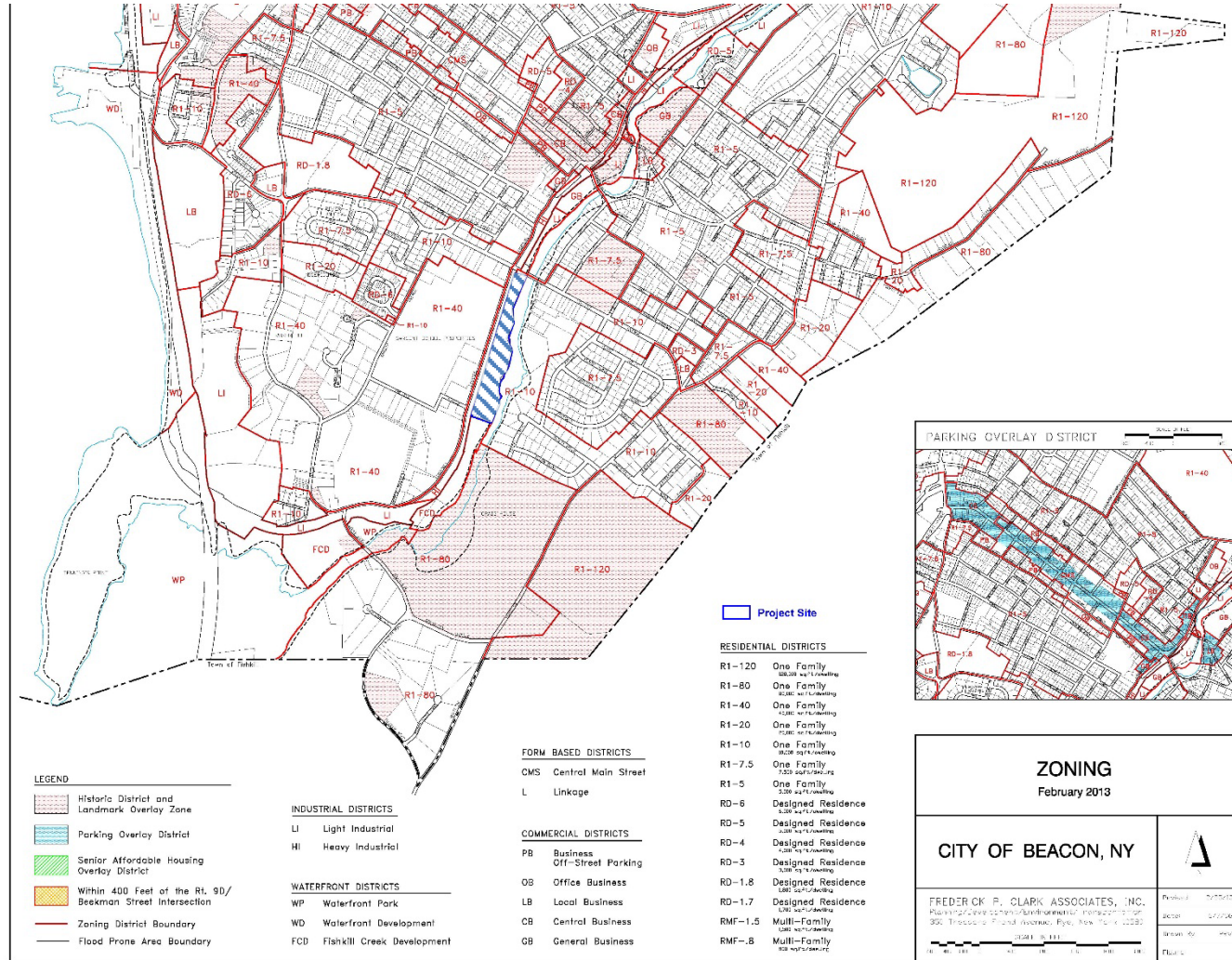
Chazen Engineering, Land Surveying, and Landscape  
Architecture Co., DPC

Beacon City Council Meeting  
March 10, 2014

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# City of Beacon, NY Zoning Map



# Aerial Overview Bing Map - 2004



# South End of Project Aerial Bing Map - 2004



# Central Portion of Project Aerial Bing Map – 2004



# North End of Project Aerial Bing Map – 2004



# Wolcott Avenue Aerial Bing Map - 2004



# Site Photos Prior to Demo





# Site Photos Prior to Demo



# Site Photos Prior to Demo



# Site Photos Prior to Demo



# City Council Work Shop Site and Project Features



- ▶ Define North–South Axis
  - Benefits to Residence and Public–Greenway Trail
- ▶ East– West
  - Varying depths, range from approximately 105’ to 235’
  - Site gets wider to the south
- ▶ Tioronda Ave – MTA
  - Length of access drive from the CL of Tioronda Ave to CL of RR tracks of:
    - Proposed Drive: 140 LF
    - Alternate North Drive: 50LF

# City Council Work Shop



## ▶ Site Slope Aspect – Direction

- North to South,
  - Emergency Access to Wolcott Ave. at a 10 % Grade
- West to East
  - Site sits below the RR track
  - Predevelopment area is adjacent to the top of the Fishkill Creek slope of approximately 10– 16 feet in height

# City Council Work Shop

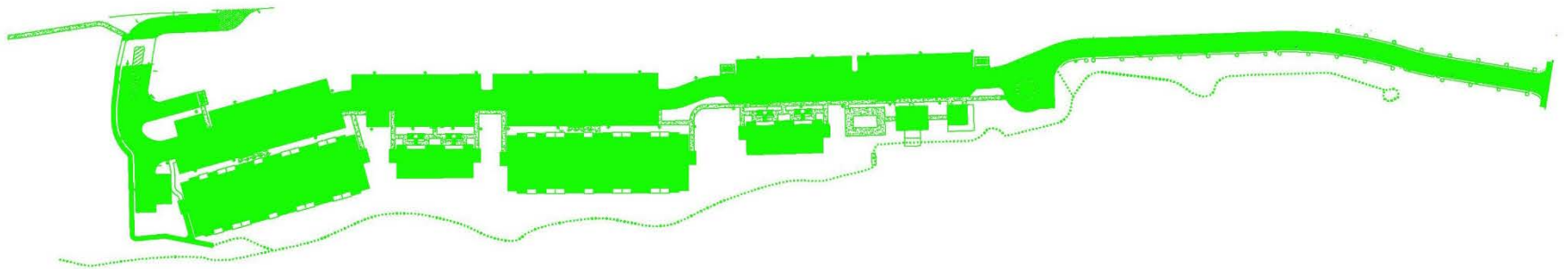


- Design Opportunity – Limit disturbance at the top of the Fishkill Creek Bank and provide:
  - Better Greenway Parkland experience
  - Save more existing Vegetation
  - Keep grade of Buildings lower relative to Tioronda Ave Grades
  - Keeps parking below Tioronda Line of Sight

# Google 2006 Aerial with Layout

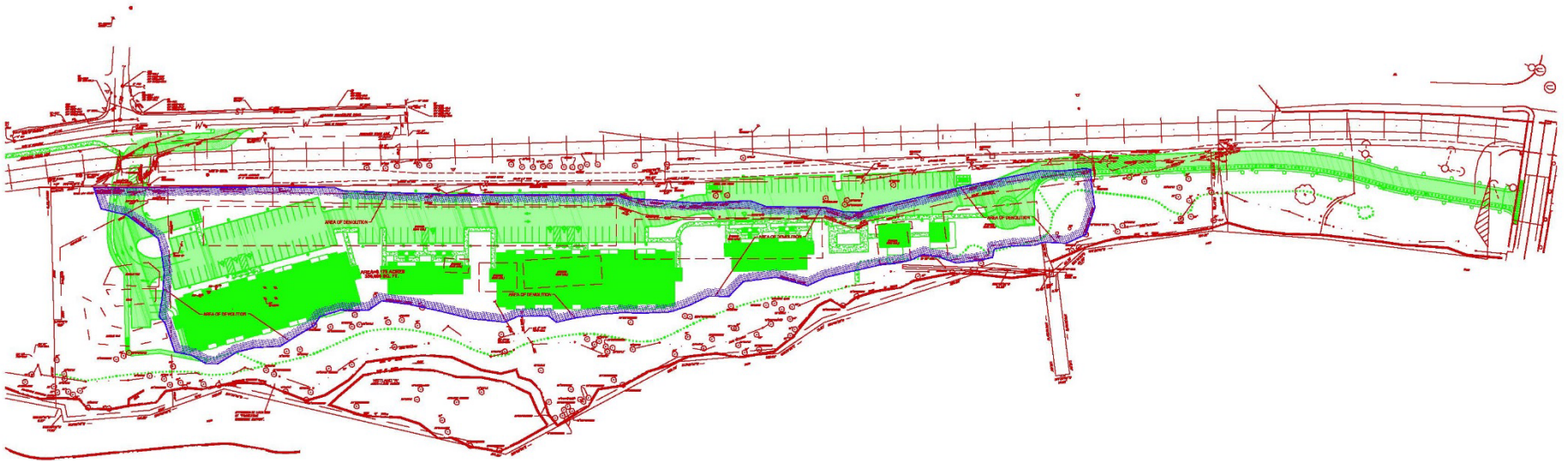


# Site Impervious surface



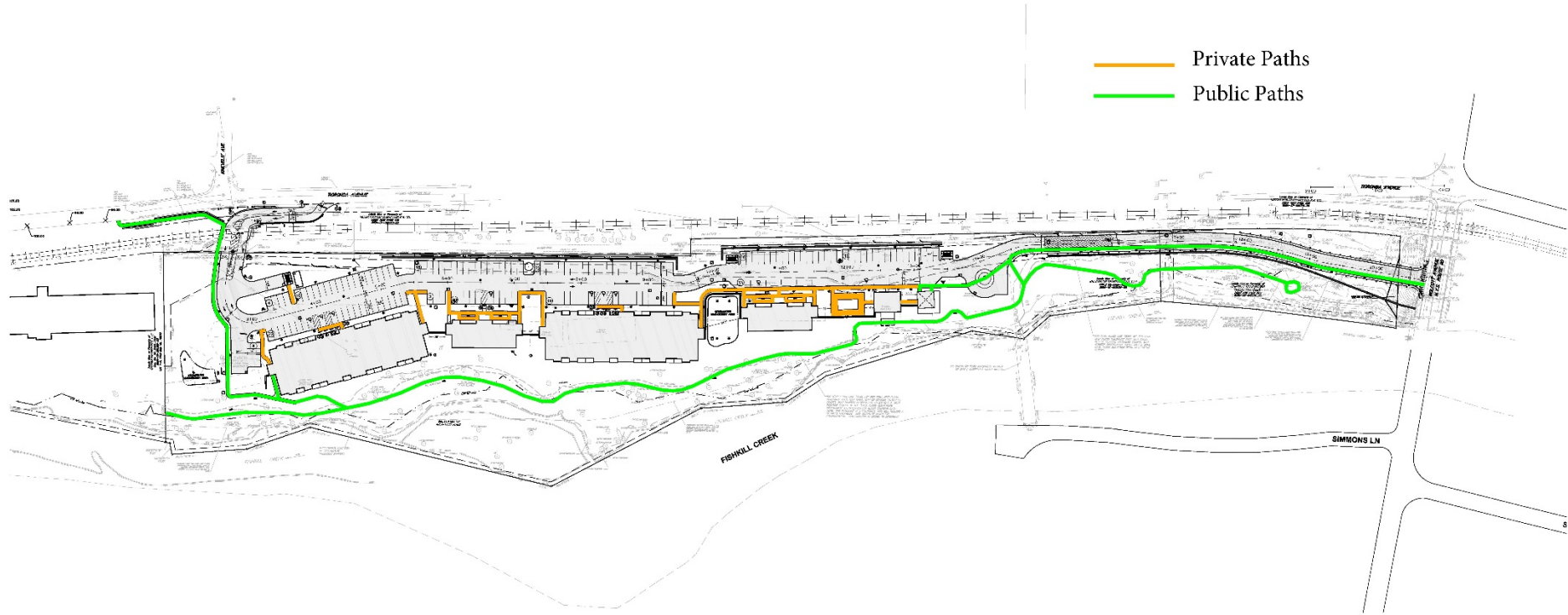


# Site Impervious surface with Limit of Demolition



# Public and Private Pedestrian Walks

- Private Paths
- Public Paths



# Proposed Layout



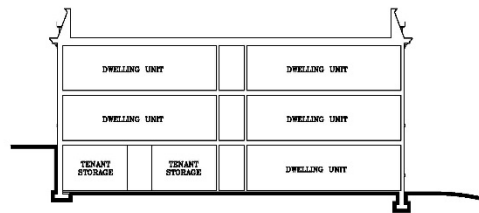
# Proposed Architecture



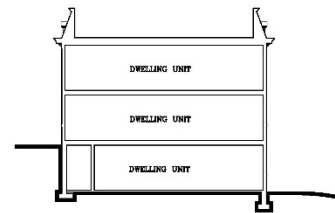
WEST ELEVATION - BUILDINGS 100 & 300  
SCALE: 1/8" = 1'-0"



WEST ELEVATION - BUILDINGS 200 & 400  
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SCHEMATIC SECTIONS - BUILDINGS 100 & 300  
SCALE: 1/8" = 1'-0"

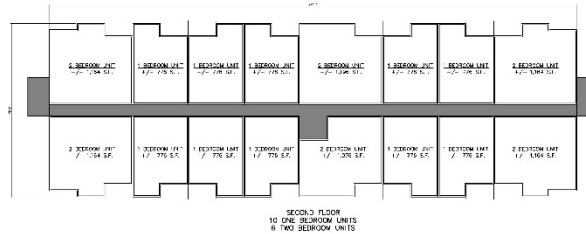
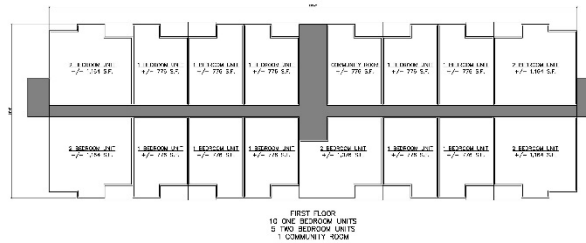
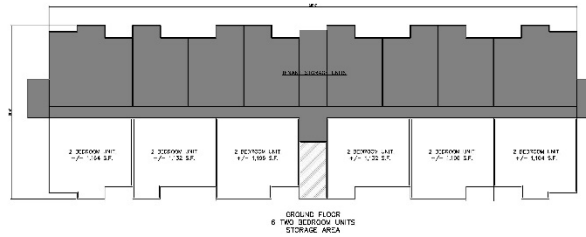


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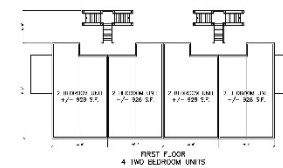
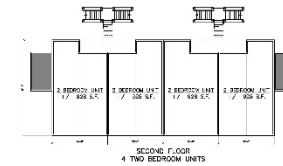
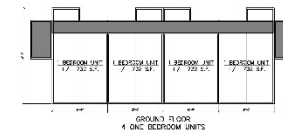
BEACON 248 DEVELOPMENT, LLC  
TORONDA AVENUE - CITY OF BEACON, NEW YORK

LISCUM MCCORMACK VAN VOORHIS  
ARCHITECTS ■ PLANNERS ■ INTERIORS  
631 CANTON STREET  
POUGHKEEPSIE NY 12601

# Unit Types and Areas



BUILDING 100  
&  
BUILDING 300



BUILDING 200  
&  
BUILDING 400

# City Council Work shop

## Topic – Location of Main Entrance

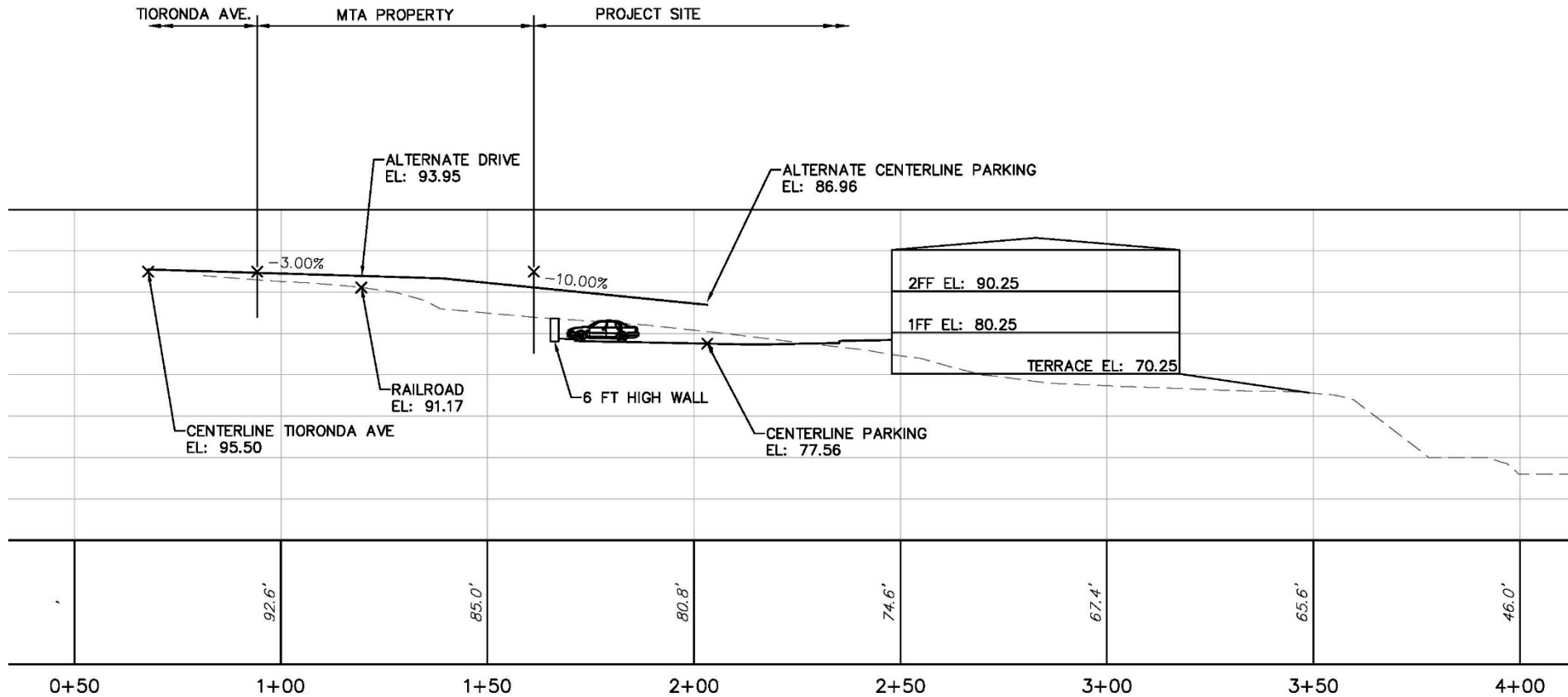
- ▶ Is the project main access on Tioronda in the Right Location?
  - YES – Historical location
  - YES – Widen to 22 feet, exists now as 14 feet
  - YES – This access allow for vehicular stacking from the stop bar to the RR track
  - YES – This access allows for the proper grade transition to the CL of Tioronda Ave
  - YES – This access allow for WB 50 and Emergency Vehicle turning radius
  - YES – This access allow an additional 150 LF of driveway to make up grade

# City Council Work Shop

## Topic – Location of Main Entrance

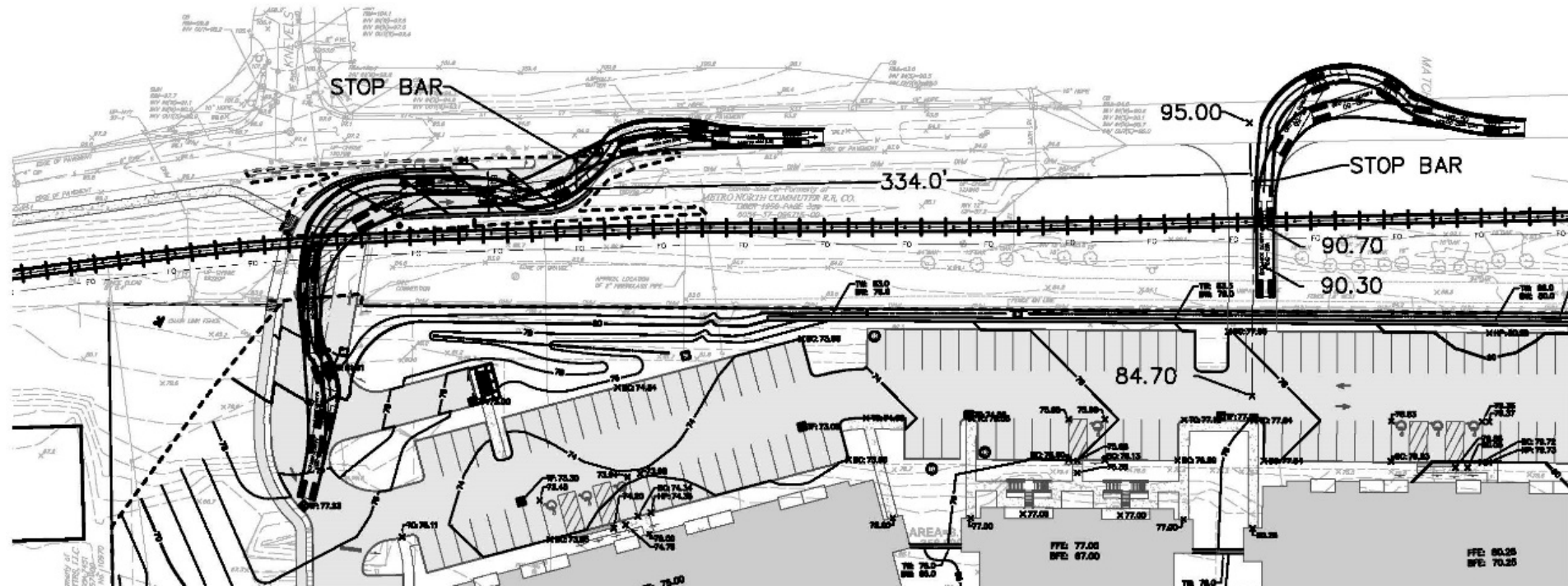
- ▶ Should the primary access be moved north to accommodate the right in from Tioronda Ave and the left turn out from the project site?
  - NO – the traffic movements are NOT essential and NOT desired
  - NO – the grade does not work from the CL of Tioronda Ave to the RR track
  - NO – The grades do not meet the grading of the parking lot
  - NO – the layout does not support a WB 50 truck

# CL Road Profile of Alternate Access





# Alternative Access: WB-50

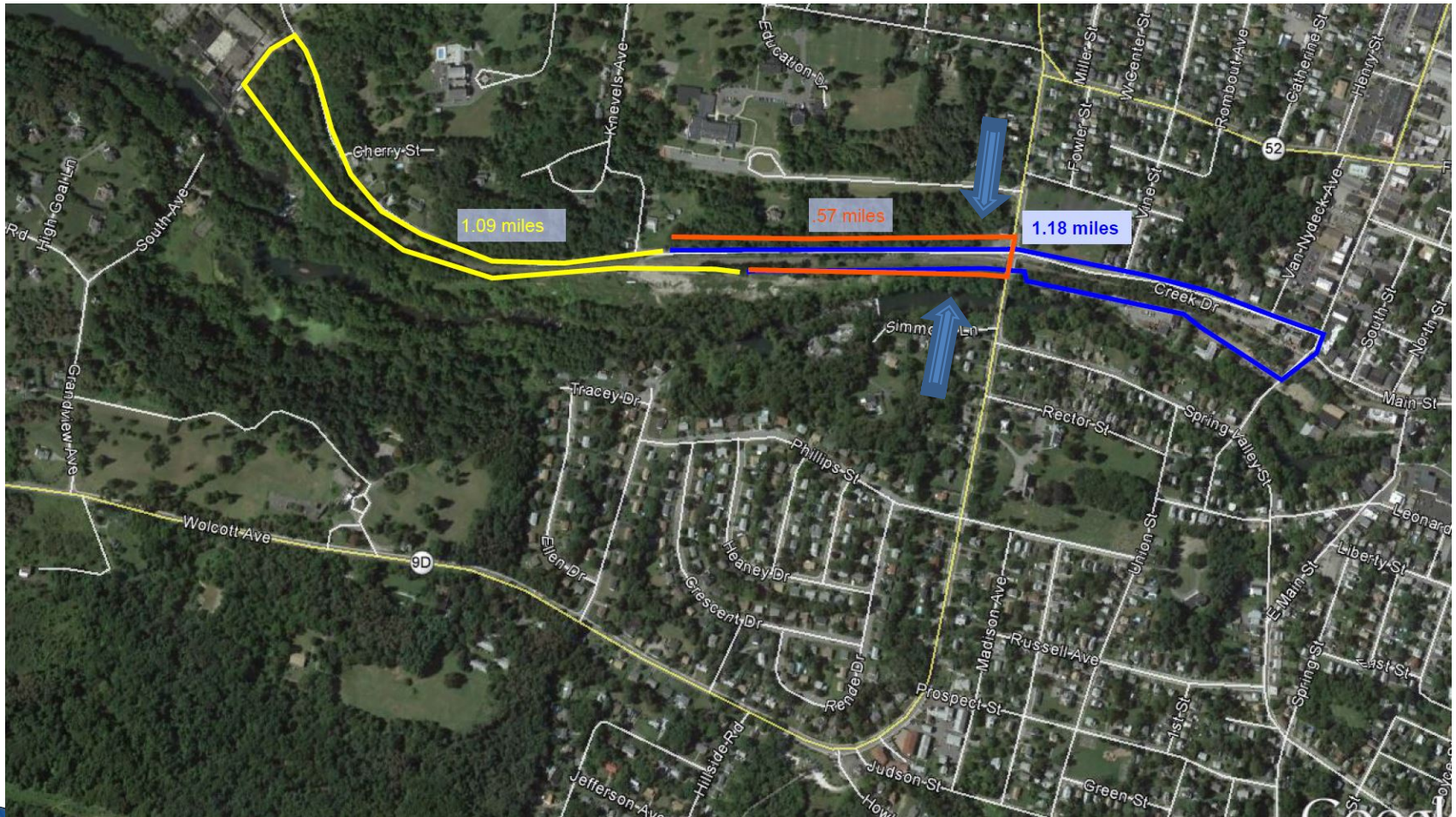


# City Council Work Shop

## Location of Emergency Access

- ▶ Is EA from Wolcott Ave, as proposed, the BEST solution?
  - YES – Shortest Route
  - YES – Quickest Route
  - YES – No adverse impact
  - YES – EA and Greenway Trail linkage
    - Under the Wolcott Bridge
    - RR Crossing Further North
    - Through Adjacent Properties
  - Profile of Existing and Proposed Grades
  - Visual Simulation of Emergency Access Drive
  - EA for adjacent parcels

# Emergency Travel Routes, Photo shots



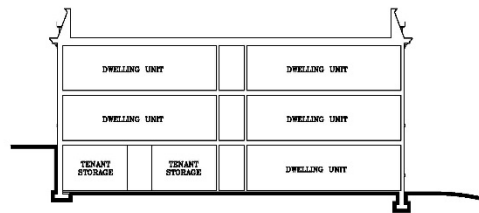
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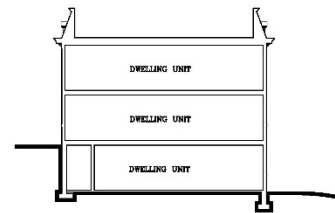
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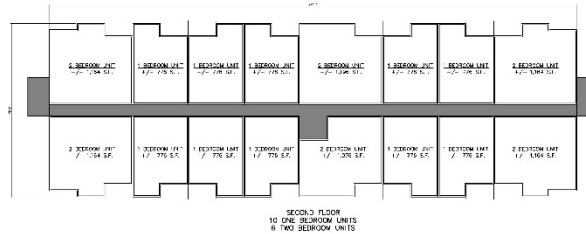
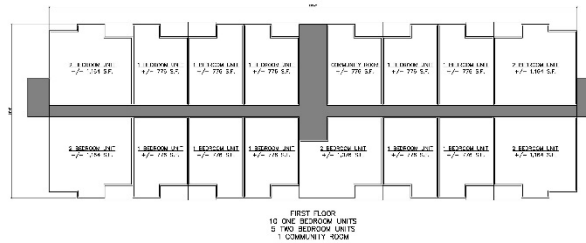
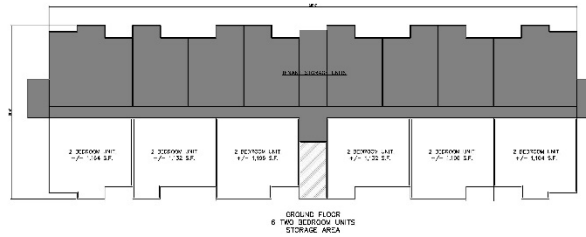


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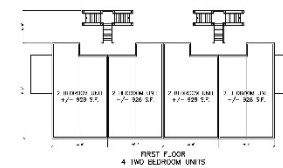
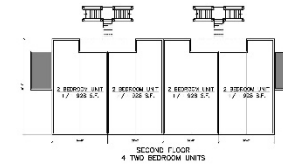
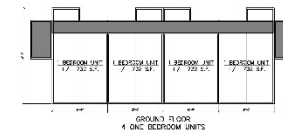
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# Photo Simulation View 1

## Knevels Ave



# Photo Sim Image “ghosted”



# Photo Simulation View 1





# Photo Simulation View 2, Tioronda Ave



# Photo Simulation View 2



# View West From Tioronda Ave



# View West From Simmons Street to Tioronda Ave.



# Emergency Access Photo Simulation



# City Council Work Shop, Traffic

- ▶ **Existing traffic**
- ▶ Tioronda is extremely light – peak hour 31 total – 15 sb/16 nb – not during commuter peaks
- ▶ **Travel Scenarios and Analysis**
  - 4 different travel scenarios – all provided good to excellent LOS
  - 2 preferred – other two dumped traffic onto Walcott directly from the site
  - Planning board recommended scenario 1 – all in and outs at Tioronda driveway
- ▶ **Turn restrictions**
  - No rights from Tioronda into the site
  - No lefts from the site onto Tioronda
  - Not movements that will not be made based on volumes
    - If one is attempted, will learn and not attempt in the future

# City Council Work Shop, Traffic

## ▶ Turn restrictions

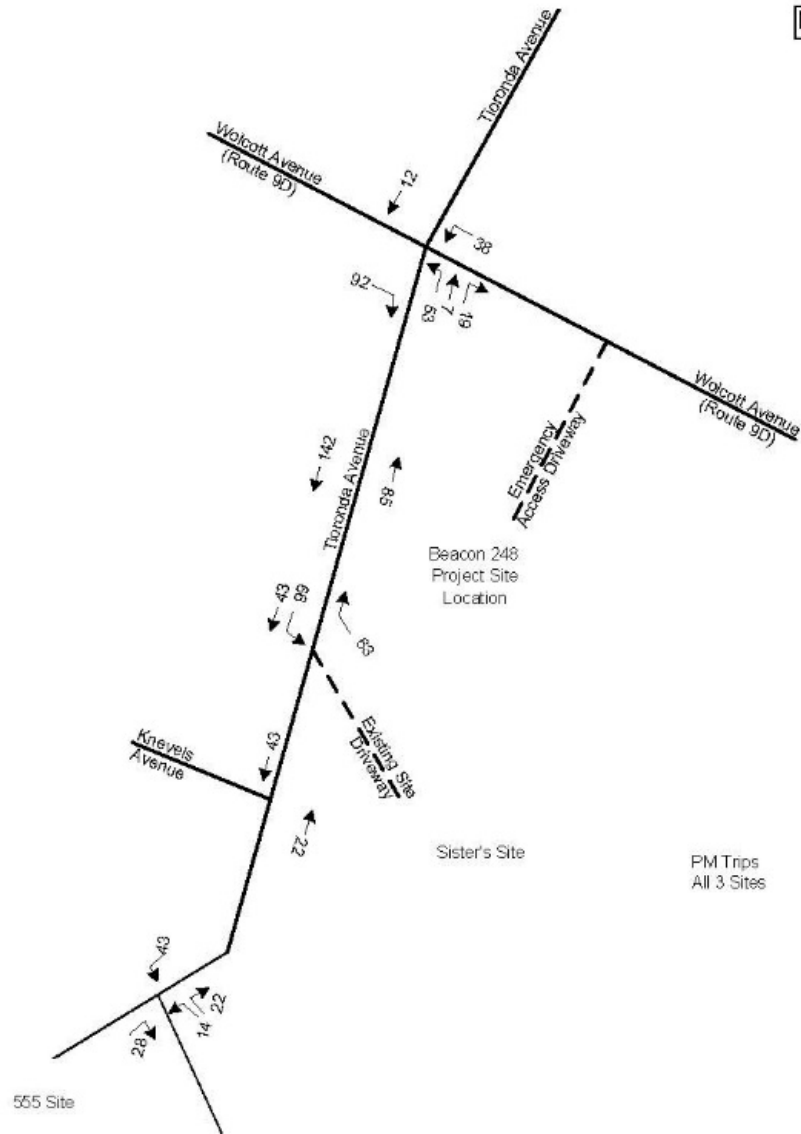
- No rights from Tioronda into the site
- No lefts from the site onto Tioronda
- Not movements that will not be made based on volumes
  - If one is attempted, will learn and not attempt in the future
  - Restrictions will
    - Remove unsafe movements
    - Keep traffic off of local streets such as Knevels, Sargent, and South Avenues

# Trip Generations – Tioronda Ave. Beacon 248, Sister’s and 555 Parcel

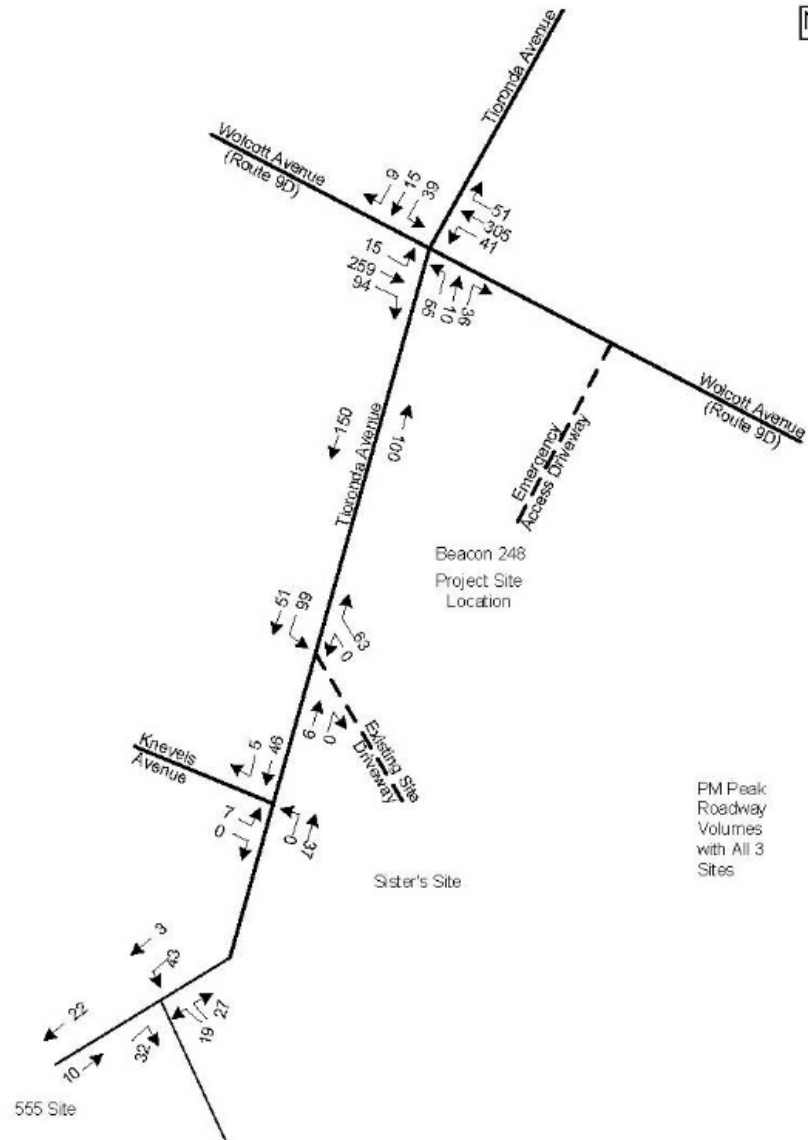
Parcel	Land Use	AM Peak Hour Volumes		PM Peak Hour Volumes	
		Enter	Exit	Enter	Exit
Beacon 248 Project	Land Use – Apartments – 100 units	11	42	47	26
Sub-Total		11	42	47	26
Sister’s Parcel	Land Use – Apartments – 73 units	8	32	38	20
	Land Use – Specialty Retail Center – 4KSF	7	9	14	17
Sub-Total		15	41	52	37
555 Parcel	Land Use – Townhouses – 200 Units	15	75	71	35
Sub-Total		15	75	71	35
All Three Parcels Total		41	158	170	98



# Trip Distribution: PM Trips, All 3 Sites



# Roadway Volumes with All 3 Sites



PM Peak  
Roadway  
Volumes  
with All 3  
Sites

# Intersection Operation with All Three Sites

